

51 Traffic and Safety Overview

Corridor Overview



Southern Termini = **South City Limits**
 Northern Termini = **North Point Drive**

Project Length ≈ **3 miles**



2017 AADT = **9,000 to 13,700**
 2045 Forecast AADT = **10,300 to 15,700**

(AADT= Average Annual Daily Traffic)

Safety Evaluation

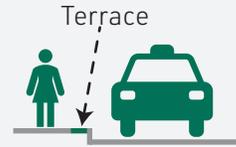
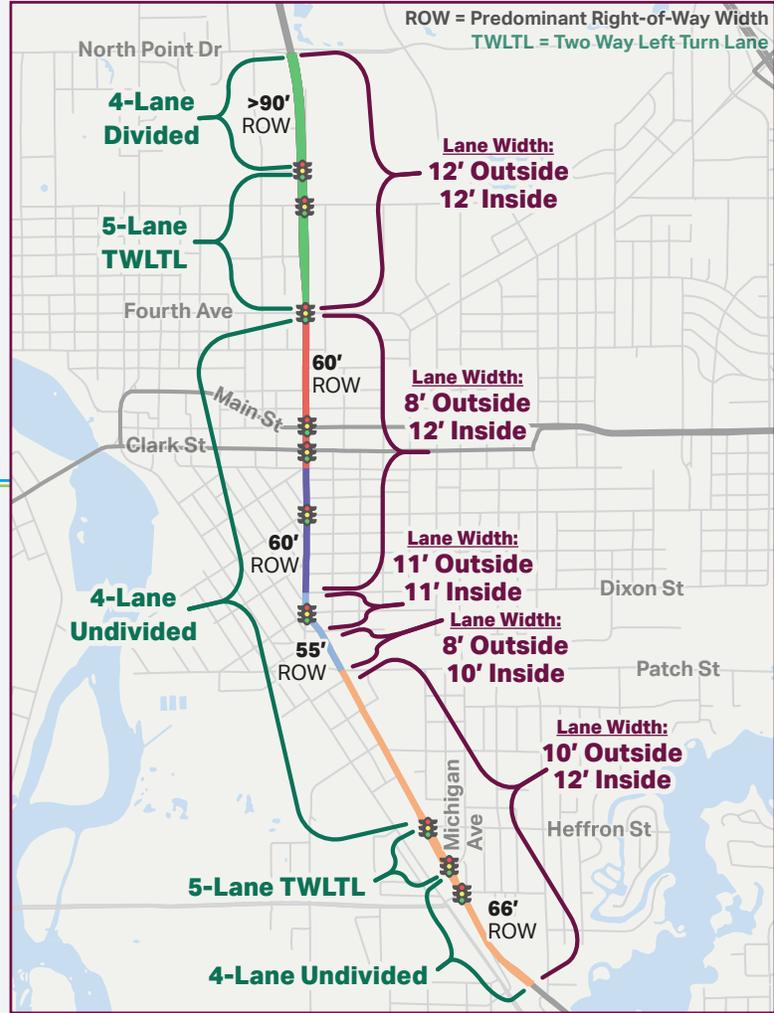
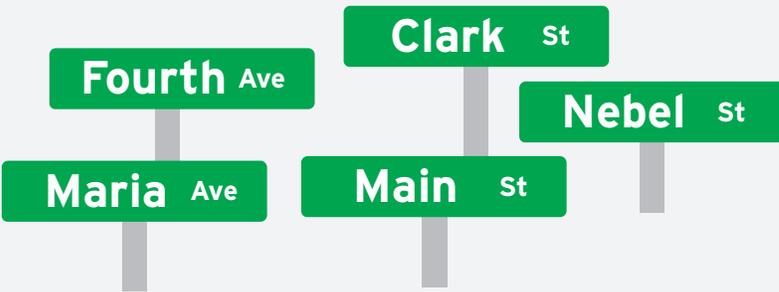


Business 51 north of Ellis St. has a **higher crash rate** than **95%** of similar roads in WI

Least Dangerous Wisconsin Roads

Most Dangerous Wisconsin Roads

Five major intersections along Business 51 have crash rates near or greater than 1.0. An intersection crash rate above 1.0 is an indication safety improvements are needed.



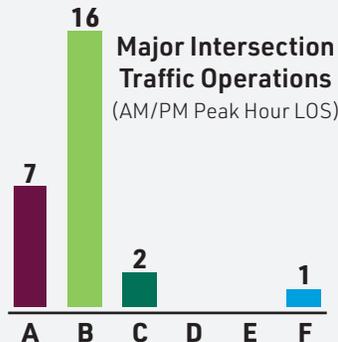
Narrow terraces (area between sidewalk and curb) do not buffer pedestrians from traffic.

Existing lane widths do not provide sufficient space for shared bicycle accommodations.



Traffic Evaluation

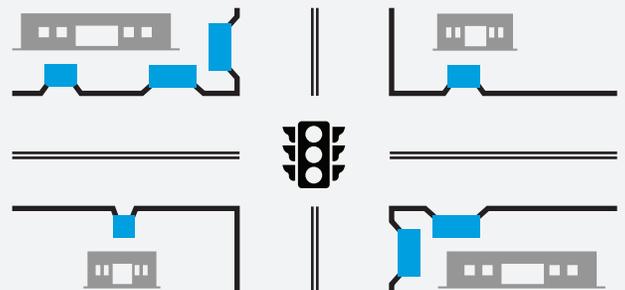
Overall traffic volume has been decreasing since 2005.



Most major intersections currently operate at Level of Service (LOS) C or better, indicating less than 35 seconds of delay per intersection.

Access Evaluation

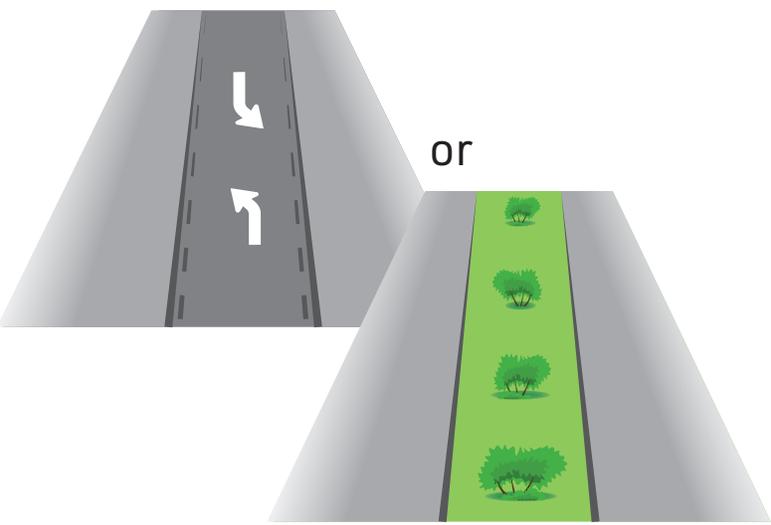
Access points located too close to intersections can be safety problems due to the number of conflict points in a small area.



Too many access points in commercial areas can also lead to safety problems.

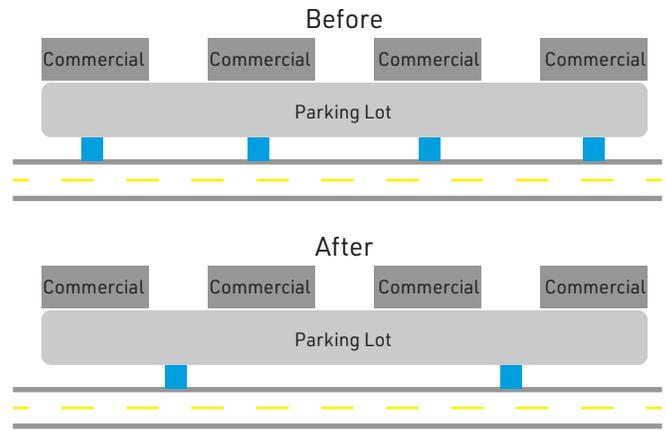
Safety Recommendations

Separate traffic by creating a divided roadway.



Access Recommendations

Consolidate access points.



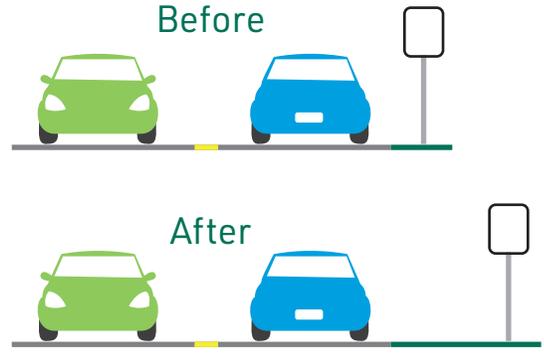
Reduce the number of access points in commercial areas and near intersections where possible.

Improve pedestrian, bicycle, and bus accommodations.



The first priority should be safe and comfortable conditions for pedestrians.

Reduce the number of fixed objects near the roadway.



Fixed objects such as traffic signals, utility poles, and signs are located close to the roadway due to a narrow terrace. A wider terrace would allow these objects to be placed further from the roadway which would reduce crash potential.

Traffic Recommendations

Travel time is largely dependent on delay caused by signalized intersections. Adding turn lanes at signalized intersections will improve traffic operations and reduce crash potential.

